

Marina Landing

(AMENDED)

PLANNED DEVELOPMENT HANDBOOK

PREPARED FOR:

**CITY OF SPARKS
AND
SPARKS MARINA RV PARK, LLC**

ADOPTED BY SPARKS CITY COUNCIL - AUGUST 26, 2002
FIRST AMENDMENT - MAY 24, 2004
SECOND AMENDMENT – JANUARY 9, 2012
THIRD AMENDMENT – MAY 2016

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CHAPTER ONE - PROJECT DESCRIPTION

I. INTRODUCTION

The Marina Landing project was approved as a planned development with adoption of Ordinance 2151 by the City Council of the City of Sparks on 26 August 2002. The property owner and developer as of August 20, 2003 proposed modifications to the approved plan, including identification of lands to be incorporated into the project and modifications to certain plan details. On January 12, 2004, and January 9, 2012 the Sparks City Council approved amendments to the Marina Landing Planned Development, and this document has been prepared to incorporate approved amendments. This document titled Marina Landing Planned Development Handbook supercedes the planned development approval of August 26, 2002.

All properties to be incorporated into the Marina Landing Planned Development are owned by Sparks Marina RV Park, LLC, a Nevada Limited Liability Company, members William A. Manke and Lavon T. Manke as Co-Trustees of The William A. Manke Family Trust. A listing of properties to be incorporated into the planned development, identified by Washoe County Assessor parcel numbers, follows. Current City of Sparks zoning and area for each parcel are also noted. Legal descriptions are included as an appendix.

Marina Landing properties:
APN 037-020-67, 13.40 acres, existing zoning NUD,
APN 037-020-60, 3.86 acres, existing zoning NUD,

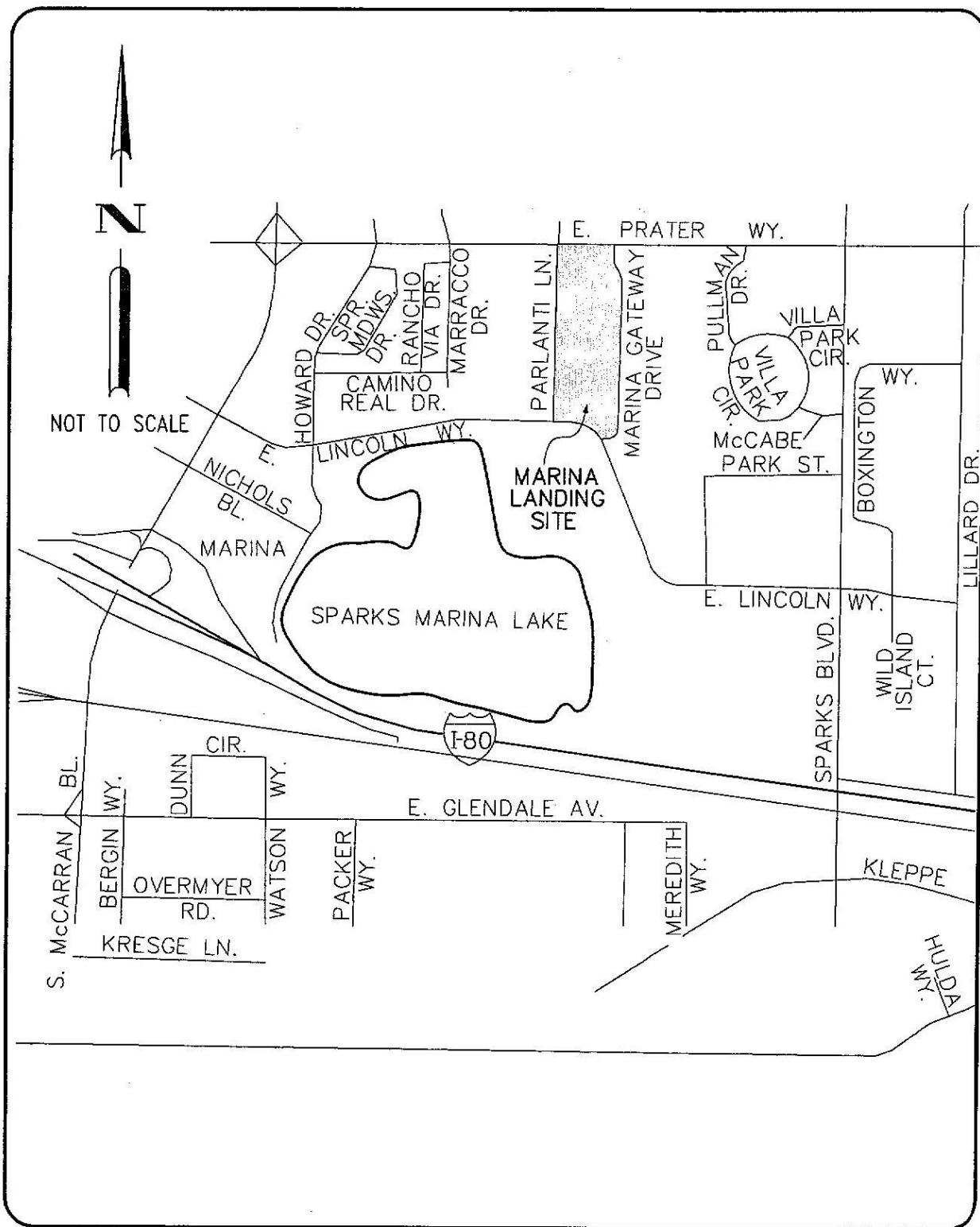

Total area, 17.26 acres.

Proposed land uses for Marina Landing include a recreational vehicle park resort, a commercial component and an office building. The plan is intended to complement the total Sparks Marina development by providing resort standard recreational vehicle rental spaces, commercial development to provide service to the RV Park occupants *and* to neighboring residential areas and office development as an integral part of Sparks Marina and surrounding neighborhoods.

II. PROJECT LOCATION

The Marina Landing properties, totaling ±17.26 acres, are bounded by the north line of Lincoln Way on the south, the south line of Prater Way. To the east, Marina Landing is bordered by APN 037-020-56 & 66. To the west, Marina Landing is bordered by the Sierra Royal Mobile Home Subdivision, a mini storage complex. Parlanti Lane, a private roadway providing access to the Sierra Royal Mobile Home Subdivision and to the mini storage complex, is within the Marina Landing project at the west boundary of the properties and will continue to provide access to the mobile home subdivision.

See Vicinity Map, Figure 1.

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MARINA LANDING

VICINITY MAP

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III. PROJECT GOALS AND POLICIES

To achieve a quality development that will benefit the City of Sparks, the following goals and policies have been adopted for Marina Landing. These goals and policies will be used to guide the development of Marina Park while fostering a consistent design throughout the project. The goals and policies have been designed to reflect creativity and detail consistent with the Marina Area Plan and NRS 278A.020.

Goal 1: *Enhance economic development in the Marina District through the use of diverse building architecture and design.*

The Marina Landing project will be developed to encourage commercial uses complementary to the function of the lake as well as recreational opportunities that take advantage of the public investment. The architecture will reflect the theme consistent with the Marina District.

Goal 2: *Provide a variety of housing opportunities with diverse housing types and prices.*

The Marina Landing project provides RV parking, but the proposed land uses will complement the residential uses in the surrounding neighborhoods and provides buffering for adjoining residential lands.

Goal 3: Encourage *high quality development throughout the Marina district thus creating a distinct image for the City of Sparks and the Marina Park.*

Through design, the Marina Landing Planned Development will complement the existing regional park amenity. A portion of the development will provide transitional buffering / screening treatments for protection of neighboring areas. The project will also create design connections to the Marina regional development plan through street furniture, lighting, building color, etc. The nautical theme shall be used consistently throughout Marina Landing, contributing to the distinctive image of the Sparks Marina District.

Goal 4: *Allow for new uses that are compatible with existing adjacent residential neighborhoods.*

Marina Landing Planned Development is a transitional development between the existing land uses to the north and Sparks Marina Park. It is envisioned to be a mixed use development that creates land use buffers between the existing and proposed development. Through design, the development will include nautical theme elements. Marina Landing will incorporate an 8 foot decorative masonry wall with buffering landscaping for the Recreational Vehicle Park. The Buccaneer Landing commercial development at the northern edge and adjoining Marina Gateway Drive is the northern gateway to the Sparks Marina Park District.

Goal 5: *Enhance opportunities for recreational use surrounding the lake.*

The Marina Recreational Vehicle Park will support the lake and produce park visitors for destination recreation purposes. The Marina Landing Plan provides pedestrian movement by sidewalks along its frontage on Lincoln Way and Marina Gateway Drive. The recreational vehicle park will also have amenities for park guests such as interior recreational areas, a swimming pool, horseshoe pits and other recreational activities.

Goal 6: *Provide public access to the lake and recreation facilities.*

Marina Landing will provide a system of walkways for pedestrian movement to the lake for both the users of Marina Landing and surrounding area.

Goal 7a: *The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing housing of all types and design.*

Marina Landing is not a housing project rather a commercial, office and recreational vehicle park designed to complement the Sparks Marina Area. The commercial and office features are complementary to surrounding neighborhoods.

Goal 7b: *The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing necessary commercial and industrial facilities conveniently located to that housing.*

Marina Landing will be a mixed use development that incorporates both commercial and office land uses, conveniently located with respect to surrounding neighborhoods and to other elements of the Sparks Marina region.

Goal 7c: *The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing the more efficient use of land and public or private services.*

Marina Landing will be an efficient use of a mixed land use development.

Goal 7d: *The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing changes in technology of land development so that resulting economies may be made available to those who need homes.*

Marina Landing is a proposed planned development responding to the recent development of the Marina District.

Goal 7e: The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing flexibility of substantive regulations over land development so that proposals for land development are disposed of without delay.

Marina Landing is a proposed planned development which has established design standards that can expedite the review process of future development through the use of Site Plan Review.

Following are Planned Development findings for Marina Landing.

PD1 The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing for housing of all types and design.

Marina Landing is not a housing project but proposed land uses are complementary to surrounding residential neighborhoods and to other elements of the Sparks Marina regional development.

PD2 The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing for necessary commercial and industrial facilities conveniently located to the housing.

Although Marina Landing does not provide housing development, proposed commercial and office clients are complementary to and conveniently located with respect to existing housing.

PD3 The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing for the more efficient use of land and public or private services.

PD4 The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing for changes in technology of land development so that resulting economies may be available to those in need of homes.

Marina Landing does not provide directly for those in need of homes but is complementary to the Sparks Marina regional development.

PD5 The plan is consistent with the objective of furthering the public health, safety, morals and general welfare by providing for flexibility of substantive regulations over land development so that proposals for land development are disposed of without undue delay.

PD6 The plan does depart from zoning and subdivision regulations otherwise applicable to the property, and these departures are in the public interest for density.

The plan requires re-zoning to NUD in order to provide for adoption of the planned development. The departures are consistent with Sparks Marina regional development for density.

PD7 The plan does depart from zoning and subdivision regulations otherwise applicable to the property, and these departures are in the public interest for bulk.

The plan requires re-zoning to NUD in order to provide for adoption of the planned development. The departures are consistent with Sparks Marina regional development for bulk.

PD8 The plan does depart from zoning and subdivision regulations otherwise applicable to the property, and these departures are in the public interest for use.

The plan requires re-zoning to NUD in order to provide for adoption of the planned development. The departures are consistent with development concepts for the Sparks Marina District and are in the public interest for use.

PD9 The ratio of residential to non-residential use in the planned development is:

Residential 0 percent, Non-residential 100 percent.

PD10 Common open space in the planned development exists for what purpose, is located where within the project, and comprises how many acres (or what percentage of the development site taken as a whole).

Common open space is not provided in the Marina Landing project, but amenities within the Recreational Vehicle Park provide recreational and open space opportunities for occupants.

PD11 The plan does not provide for the maintenance and conservation of the common open space by what method.

Because there is no common area open space, maintenance and conservation is not provided. Within the RV Park the owner will provide directly for maintenance and operation of indoor and outdoor common area facilities.

PD12 Given the plan's proposed density and type of residential development, the amount and / or purpose of the common open space is determined to be adequate.

PD13 The plan does provide for public services. These provisions are adequate.

PD14 The plan does provide control over vehicular traffic.

PD15 The plan does provide for the furtherance of access to light, air, recreation and visual enjoyment.

PD16 The relationship of the proposed planned development to the neighborhood in which it is proposed to be established is beneficial.

PD17 To the extent the plan proposed development over a number of years, the terms and conditions intended to protect the interests of the public,

residents and owners of the planned development in the integrity of the plan are sufficient.

PD18 The project, as submitted and conditioned, is consistent with the City of Sparks Master Plan.

PD19 The project is consistent with the surrounding existing land uses.

PD20 Public notice was given and a public hearing held per the requirements of the Sparks Municipal Code.

PD21 Modification of Marina Landing Planned Development furthers the interest for the City and the residents and preserves the integrity of the plan.

IV. SURROUNDING LAND USES / VICINITY

See Figure 2, Surrounding Land Uses Map.

Marina Landing is part of the Sparks Marina Park District, currently in process of being developed under a comprehensive, multiple use plan. Major infrastructure components for the regional development plan have been completed, including drainage, sanitary sewer, utilities main extensions and major public streets, Lincoln Way and Marina Gateway Drive. To the south and southeast of Marina Landing are the Sparks Marina Lake and, adjacent to the lake, a planned development designated Marina Village.

Marina Village, a planned development, provides a hotel, a marina, an apartment complex and a single-family development with a canal, docks and a harbor like atmosphere. Development of Marina Village is in process in 2003.

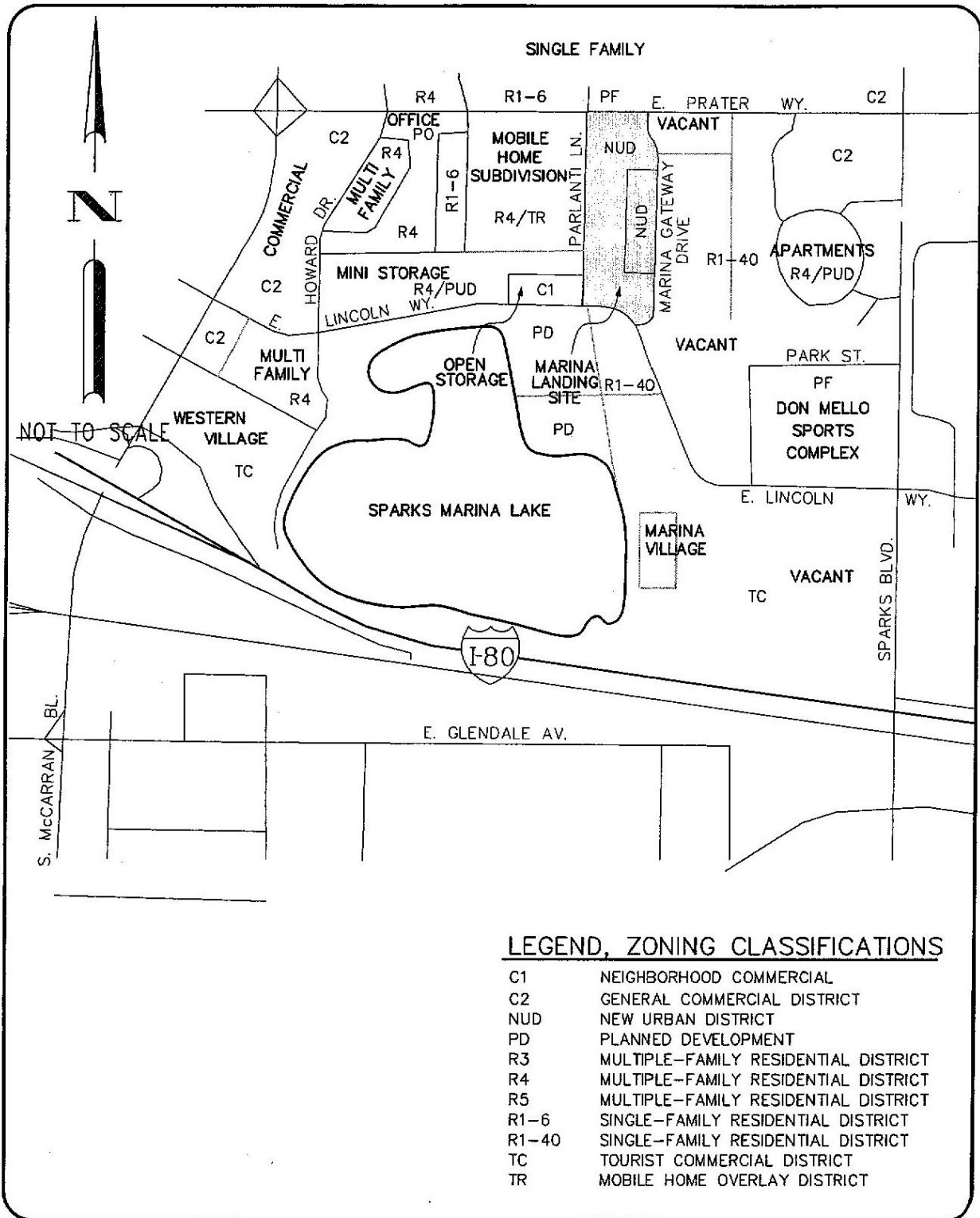
To the east of Marina Landing, lands are vacant. The vacant properties, known as the Ghiggeri Ranch, will be part of a future planned development that may include office development, single and multi-family homes, a potential City Hall site, commercial development, parks and detention facilities, etc. The Marina Landing property was formerly a part of the Ghiggeri Ranch.

Park Vista Apartments, an 840-unit apartment complex, and Don Mello Sports Complex are located farther to the east and southeast of Marina Landing.

To the north of Marina Landing, across Prater Way, are located single-family residential neighborhoods, a city park and, further east, commercial development.

The Sierra Royal Mobile Home Subdivision, consisting of 151 mobile home lots, is located west of Marina Landing and south of Prater Way. At the southwest corner of Marina Landing, the adjoining property which is vacant is owned by Marina Landing RV Park, LLC, and to the north of the vacant property is a mini storage complex owned and operated by the Manke family. Secondary access to the mini storage complex will be provided through the RV Park.

Properties to the west of the Sierra Royal Mobile Home Subdivision have been developed to include commercial, office and multi-family residential features.




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MARINA LANDING

**SURROUNDING LAND USES
AND ZONING MAP**

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V. SITE ANALYSIS

Geologic conditions and results of soils exploration are contained in a report titled Geotechnical Investigation, Proposed Marina Landing, Phase 1, Sparks, Nevada prepared by Pezonella Associates, Inc., Consulting Engineers and Geologists and dated September 4, 2002. The geotechnical report notes that there are some undesirable conditions which could affect the development of the site but concludes that the site is suitable for the intended use, provided that design and construction of improvements conform to recommendations contained in the report.

Specifically, some soils exhibit a potential for expansion with changes in moisture content. There is a relatively high ground water table, 7 to 10 feet, and near surface soils contain abundant roots and organics. These conditions can be overcome by over-excavation of expansive soils and organics and use of structural fill to support foundation loads and pavements. Design of improvements will incorporate these recommendations. Construction of the RV Park has begun under the August 2002 approval and utilizes structural fill over the site to conform to recommendations of the geotechnical consultant.

Flood Hazard studies completed by the Federal Emergency Management Agency (FEMA), Community Panel Number 32031C3011 E, September 30, 1994, indicate the site is located in or within very close proximity to Flood Hazard Zone AE, which is a special flood hazard area inundated by 100 year flooding and a base flood elevation determined to be 4,392 feet. The flood hazard is also overcome by placement of structural fill over the site in order to raise finish elevations above the flood hazard level.

The Sparks Municipal Code 20.18.050 K. L. and M. provide for special design considerations for lands that have surface slopes in excess of 10%. The Marina Landing property is essentially quite flat. There is a band of relatively steep slopes adjacent to the roadway of Parlanti Lane, but the extent of steep slopes is limited to a small percentage of the site and provisions of paragraphs K. L. and M. do not apply.

Wetlands have not been identified at Marina Landing. Seismic hazard is consistent with typical conditions in the Truckee Meadows and is mitigated by application of the Uniform Building Code and the Sparks Building Code for design of structures.

VI. PROJECT DESCRIPTION

The Marina Landing project consists of two separate land use components: a recreational vehicle park, and a commercial component. Following is a project summary.

Project Name	Land Use	Acres	Size
Marina RV Park	RV Park	±13.40 including Parlanti Lane easement and entry easement	204 RV spaces Clubhouse 2,500 sq. ft. maximum Main building (office, store, restrooms / showers and laundry) 4,600 sq. ft. maximum Four restroom / shower buildings Each 1,622 sq. ft., Total 6,488 sq. ft. Residential manager's apartment 2,071 sq. ft., including deck
Buccaneer Landing	Commercial	±3.86	±38,000 sq. ft. buildings footprint
Total		±17.26	±53,659 sq. ft.

See Figure 3, Development Plan. See Chapter 1, Section IX for development phasing.

MARINA LANDING RECREATIONAL VEHICLE (RV) PARK

The recreational vehicle park is located in the center of the proposed Marina Landing Planned Development. It is intended to be a resort for recreational vehicle travelers. The site is 13.39 acres in size and will provide 204 recreational vehicle spaces. See Figure 4 for Conceptual Site Plan. See Chapter 2, Section II for RV spaces detailed descriptions.

The RV Park will have primary access from Lincoln Way and secondary access from Prater Way via Parlanti Lane, complying with Fire Chief requirements for access. The convenience store, meeting or function room, management office, manager's apartment, swimming pool, restrooms and laundry will be clustered at the southeast corner of the RV Park in two buildings connected by a roof structure spanning across the entry driveway. The two buildings are located adjacent to the primary entrance from Lincoln Way. The convenience store will sell RV supplies, fishing tackle and equipment, food and gifts. The laundry will provide laundry facilities reflective of Washoe County District Health Department requirements. Swimming pool and picnic play area will be outdoors adjacent to **the** two buildings.

The RV Park will be surrounded by a masonry wall and landscaping. A landscape buffer will be provided adjacent to The Sierra Royal Mobile Home Subdivision to the west of existing Parlanti Lane. The landscape buffer will consist of a 6 foot high, black vinyl coated chain link fence with vines and columnar evergreens. See Chapter 2, Section VII for detailed description of landscape buffer.

Parlanti Lane will be reconfigured from Prater Way south to the entrance of Sierra Royal Mobile Home Subdivision. The existing sidewalk and curb and gutter, adjacent to the mobile home subdivision, existing masonry wall and landscaping will be maintained in place. Landscaping and masonry wall will be provided on the east side of Parlanti Lane. See Figure 16 for Parlanti Lane details including landscaping.

To the south of the entrance to Sierra Royal Mobile Home Subdivision, Parlanti Lane right-of-way will be incorporated into the RV Park. The existing masonry wall on the mobile home subdivision property line will remain undisturbed, and the existing sidewalk and curb and gutter will remain in place. The landscape buffer fence with vines and columnar evergreens will be located along the east edge of the existing curb and gutter and sidewalk. An access easement will be provided along the west boundary of the RV Park, containing the existing sidewalk, for the purpose of allowing access by representatives of the Sierra Royal Mobile Home Subdivision for maintenance of landscaping located on the mobile home subdivision property but outside of the existing masonry wall. Additional landscaping will be provided between the back of RV spaces and the landscape buffer fence.

The swimming pool and spa will be for the use of guests only. The pool / spa will be monitored with limited access.

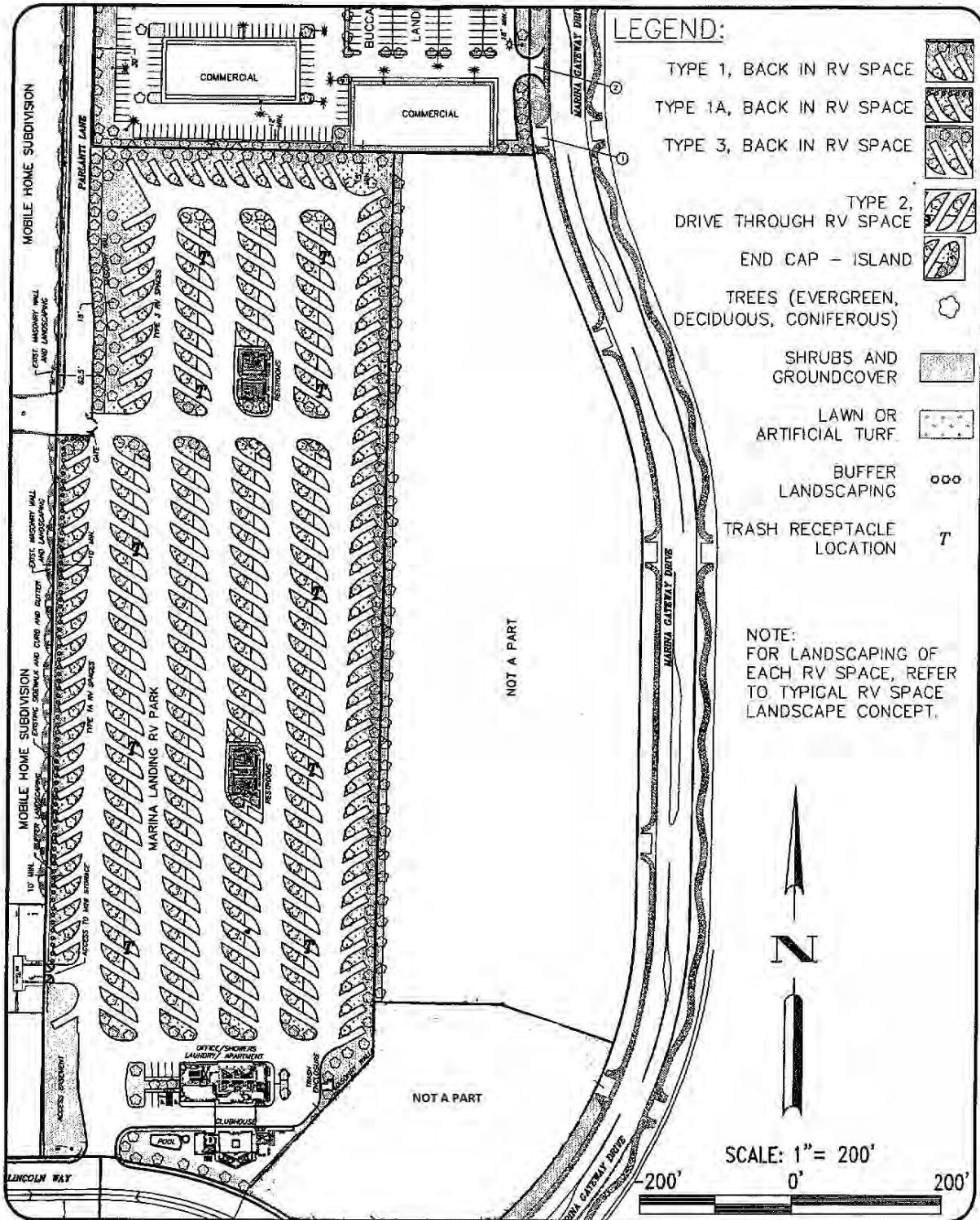
Twenty-four hour on-site management will be provided to ensure a safe environment for guests. Access to the site will be monitored, and if an electronically controlled gate at Lincoln Way is provided, the site design will include a turn-around area outside of the gate.

The Marina Recreational Vehicle Park will be a vacation resort. Permanent residents will not be permitted. Limits on the amount of time guests may stay are outlined in Sparks Municipal Code. The resort will be marketed through various recreational vehicle publications and clubs. It is anticipated that a large portion of the resort clientele will be summer travelers and will probably include a high number of retirees.

BUCCANEER LANDING

This portion of the planned development is a general / neighborhood commercial component serving as the north entrance to the Marina District. The property is approximately 3.86 acres in size and is generally rectangular in shape. The concept has three separate buildings. Refer to the Buccaneer Landing Conceptual Site Plan, Figure 5. The conceptual product mix will be a mixture of national and local retail. The total area of commercial buildings footprint is approximately 38,000 square feet. A second story could be considered within the commercial buildings, but parking availability may limit the building floor area. There may be an office component within the complex. There will be accommodations for outdoor restaurant seating accessory to the adjoining commercial tenants. The general commercial structures will be subject to the review and approval through the Site Plan Review process by the City of Sparks. The site will have two primary access points: one on E. Prater Way approximately mid-block and the second, a right-in, right-out access on Marina Gateway Drive; locations shall be to the approval of the City Engineer.

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MARINA LANDING
RECREATIONAL VEHICLE PARK,
CONCEPTUAL SITE PLAN
AND CONCEPTUAL LANDSCAPE PLAN

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VII. ARCHITECTURAL THEME

Marina Landing proposes to use the architectural theme of "nautical" that is emphasized in the Marina Area Plan. This architectural theme is consistent with the architecture of the Sparks Marina Park and future developments around the park. Examples of a "nautical theme" would be the architectural style that one would find near coastline developments and would use materials such as corrugated metal, brass, stucco and wood. The colors will be blues, whites, grays and earth tones.

VIII. INFRASTRUCTURE

SANITARY SEWER

A report titled Sanitary Sewer Design Report, Marina Landing Recreational Vehicle Park prepared by Knopf Sharp and Associates and dated May 20, 2003 contains estimates of average daily and peak flow rates for sanitary sewage produced by 204 RV units, plus clubhouse and administration building. The report provides determinations of sewage flow velocities at selected locations within the internal sewer collection system, to assure that low flow velocities conform to code requirements.

Following is a summary of estimated quantities of wastewater anticipated for Marina Landing when completed and assuming 100% occupancy for the RV Park.

Marina Landing RV Park, 204 RV spaces, clubhouse and administration building	144,300 gpd
Buccaneer Landing	<u>12,300 gpd</u>
Total	156,600 gpd

Design of sanitary sewer is based on peak flow rate. Average and minimum rates could be significant for evaluation of minimum flow velocities. Following are summaries of average daytime and peak flow rates for the project.

Average daytime flow rates:

Marina Landing RV Park, 204 RV spaces, clubhouse and administration building	19.51 gpm (.0435 cfs)
Buccaneer Landing	<u>12.85 gpm (.029 cfs)</u>
Total	32.36 gpm (.0725 cfs)

Peak flow rates:

Marina Landing RV Park	29.79 gpm (.066 cfs)
Buccaneer Landing	<u>25.69 gpm (.057 cfs)</u>
Total	55.48 gpm (.123 cfs)

Peak flow rates are significant for evaluation of impacts on capacity of public sewers. For the Marina Landing properties, sanitary sewers have been designed and constructed to provide for development consistent with the Marina Landing project.

HYDROLOGY AND STORM DRAINAGE

A report titled Hydrology Report for Marina Area SAD Phase 2 prepared by Odyssey Engineering, Incorporated, and dated January 7, 2003 is considered to be the master plan for the Marina area, including Marina Landing, and has been used as the basis for construction of storm drains under the Special Assessment District, Phase 2. The report includes the Marina Landing property in the storm drain service area.

A second report titled Hydrology and Drainage Design Report, Marina Landing Recreational Vehicle Park prepared by Knopf Sharp and Associates and dated May 14, 2003 supports design of stormwater collection features of the RV Park, relative to the master hydrology and drainage plan and provides for construction of a stormwater management system, as a part of development of the RV Park.

Consideration of development of the Marina Landing properties is included in the capacity design for the Marina District storm drain system. Additional stormwater management features will be incorporated into future development plans for Marina Landing.

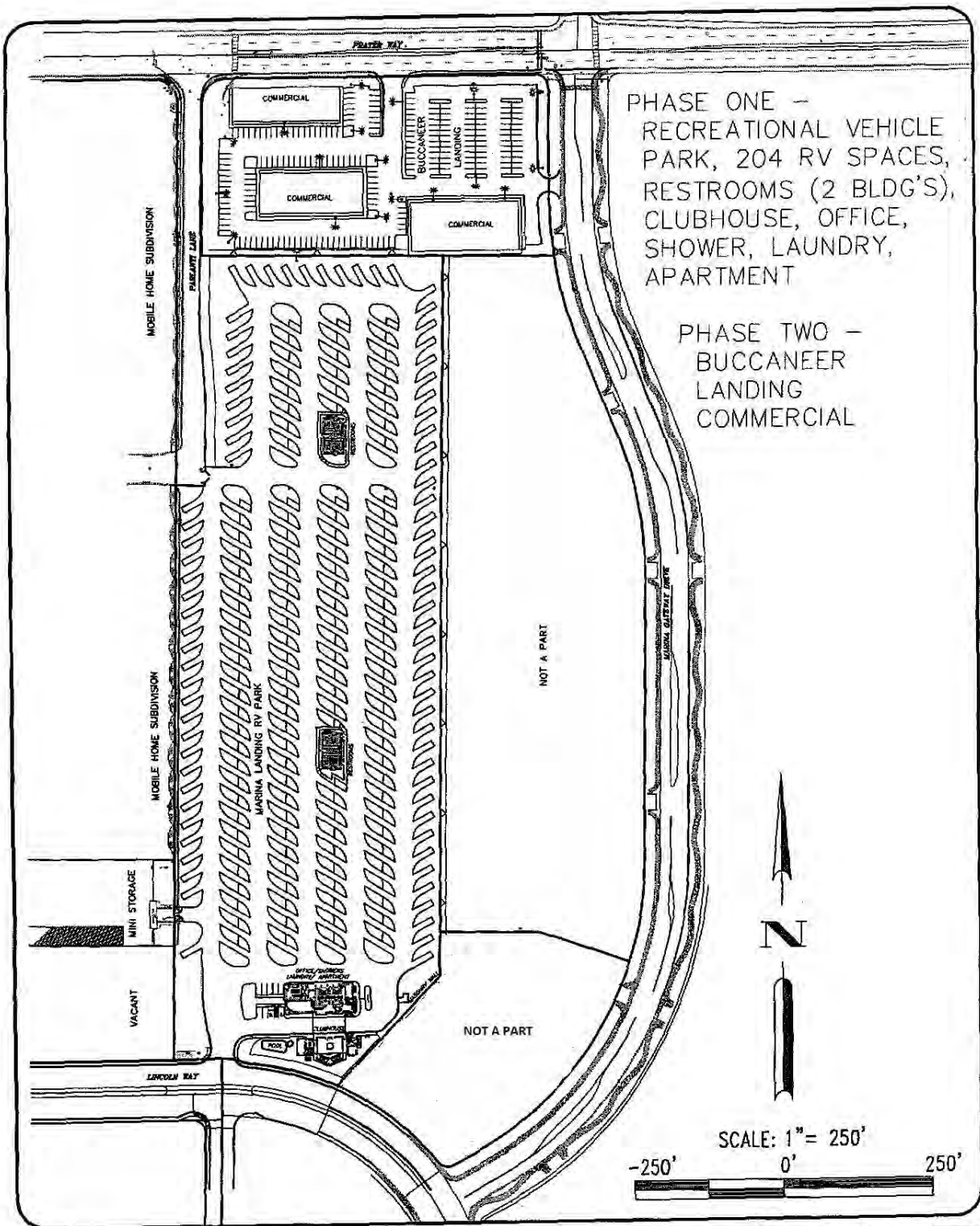
TRAFFIC ANALYSIS

Marina Landing traffic impacts were evaluated by Solaegui Engineers in conjunction with the project approval in 2002. The original traffic analysis was updated in a supplementary report dated August 21, 2003. A copy of the supplementary report is included as an appendix. Review of traffic projections based on design modifications indicates that the Marina Landing traffic impacts are not significant and are consistent with the original 2002 evaluation.

IX. PROJECT PHASING

Marina Landing will be developed in phases over a period of years. The following is an anticipated phasing schedule of development for the various uses. The phasing strategy for Marina Landing is a probable and rational sequence for development. The starting and completion dates of each component will be a function of market conditions. Marina Landing Planned Development will be developed in 2 stages. See Phasing Plan, Figure 7.

Development of the Marina Landing Recreational Vehicle Park will be Phase One, providing for initial development of 204 RV spaces. Construction of Phase One, the RV Park, began in the spring of 2003 with completion expected by April 2004. The second phase of the Marina Landing Planned Development will be Buccaneer Landing, a commercial center. It is anticipated that this development will begin construction by the spring of 2005.




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MARINA LANDING

PHASING PLAN

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X. ADMINISTRATION

This Handbook contains the Developmental Standards for the Marina Landing Planned Development which will function as the zoning for this development. The plans provided in this document are for illustrative and conceptual purposes only and are not intended for final design. The City Engineer and the Administrator shall have the responsibility to interpret standards as provided in this document, and *where* the handbook is silent, current ordinances and policies at the time of submission for permit shall apply.

The Handbook identifies the Site Plan Review process as an appropriate administrative review. The standards within this Handbook apply to and govern all new construction, remodeling, signs, walls *I* fences and landscape. All applications submitted for a permit will require prior approval of a Site Plan Review by City of Sparks.

XI. MINOR ADJUSTMENT PROCESS

Changes or refinements to this Handbook may be approved by the Administrator, when in the opinion of the Administrator, the changes or refinements do not impact the health, safety and welfare of the general public, when project design or site circumstances, etc. warrant the changes or refinements, and the intent of the Development Standards Handbook is met with approval of the changes or refinements and when the changes or refinements do not violate City Code.

Minor deviations to the plans and standards and / or guidelines may be approved by the City Engineer or Administrator provided that such changes further the Goals and Policies of the Marina Landing Planned Development and that no quantitative amount is varied more than 10 percent. Amendments to the Handbook and alterations beyond the scope of minor deviations shall be processed by the City of Sparks in accordance with local and state laws regarding planned developments. Mitigation measures may be required beyond the standards of the Handbook to address the proposed deviations.

CHAPTER TWO - DEVELOPMENT STANDARDS

I. PROJECT GENERAL STANDARDS

- Second floor residential and / or office uses will be permitted above commercial uses with proper design approved through Site Plan Review with the City of Sparks.
- Parking shall be dispersed throughout the site to avoid a mass of parking. All parking areas shall be landscaped, including canopy shade trees, and enhanced with nautical theme lighting. Only employee parking shall be allowed in the rear of buildings.
- Small outdoor plaza areas, associated with food service, outdoor retail sales or seating area for patrons, shall be provided.
- The hours of operation for commercial use shall be limited to 7:00 am. to 11:00 p.m. unless extended through the special use permit process.
- Pedestrian walkways will clearly direct pedestrians along the roadways to the intersection of Lincoln Way and Howard Drive to facilitate pedestrian crossing at a controlled intersection.
- Single-story buildings shall be setback a minimum of 25 feet from the adjacent single-family residential use, two-story buildings shall be setback a minimum of 40 feet and three-story shall be setback a minimum of 60 feet.
- Truck delivery service to the rear of buildings shall not be permitted. The buildings shall be setback a minimum of 40 feet from the rear property line. Deliveries, parking lot cleaning, refuse collection and snow removal shall be limited to the hours of 7:00 a.m. and 9:00 p.m.
- The architectural theme will connect the commercial center, the office complex and the recreational vehicle park. This will be a nautical theme as described in the Architectural Theme Section and Architecture Development Standards.
- There will be an entrance monument at E. Prater Way and Marina Gateway Drive entrance. This monument will have stone, water and plants with a nautical flavor. A public art piece in a nautical theme may serve as part of the monument.
- The perimeter wall treatment of Marina Landing shall be identical for all of Marina Landing Planned Development.

II. SITE STANDARDS

MARINA RECREATIONAL VEHICLE PARK

Permitted Uses

- Parking and temporary lodging of recreational vehicles in RV Park
- Parking of vehicles accessory to the recreational vehicles
- Convenience store (not to exceed 2,000 sq. ft.)
- General office facilities in support of underlying uses (not to exceed 1,350 sq. ft.)
- Recreational facilities for guests only
 - swimming pool
 - spa
 - pet exercise areas
 - horseshoe pits
 - picnicking areas
 - game arcade (not to exceed 200 sq. ft.)
- On-site manager living quarters
- Clubhouse (not to exceed 2,500 sq. ft.)
- RV Park maintenance and support facilities
 - freestanding bath and shower facilities
 - sanitary sewer/dump station facilities
- Temporary construction yard and trailer/ office during construction of RV Park

Uses Requiring a Special Use Permit

- Mini market with fuel pumps

General Statement, Prohibited Uses

- All uses not listed as permitted are prohibited.

Lot Characteristics

- Front yard setback for structures: 20 feet
- Side yard setback for structures: 20 feet
- Rear yard setback for structures: 20 feet
- Maximum building height: 25 feet
- See Figures 8, 9, 10, 11, 12 and 13 for RV space details.

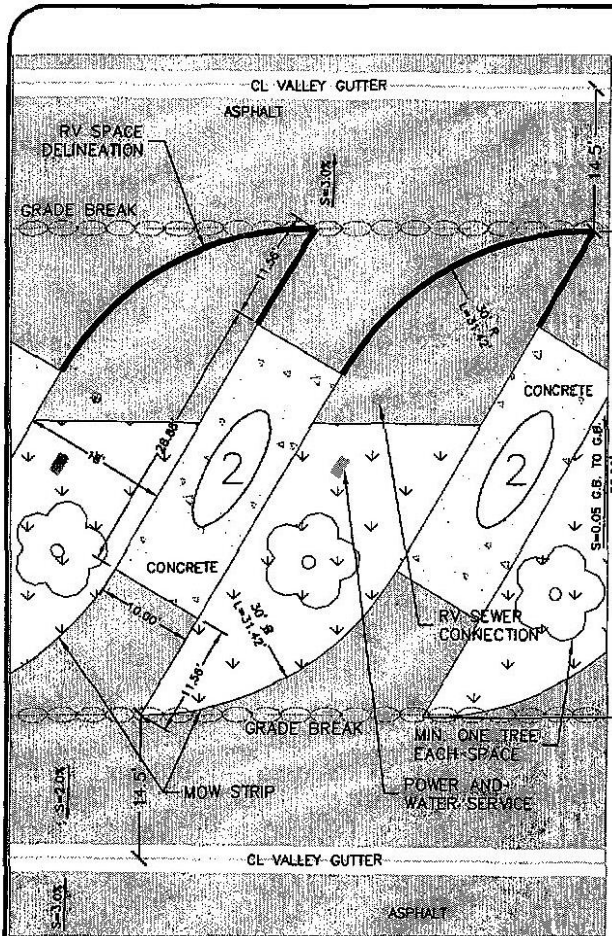
Special Conditions

- RV spaces will be arranged in a manner so that there will be a minimum of 15 feet between camping vehicles or other structures, and each camping vehicle will be at least 10 feet from any road, public walkway or exterior boundary of the property.
- Each recreational vehicle space shall have full hook-ups, including potable water, sewer and electricity.

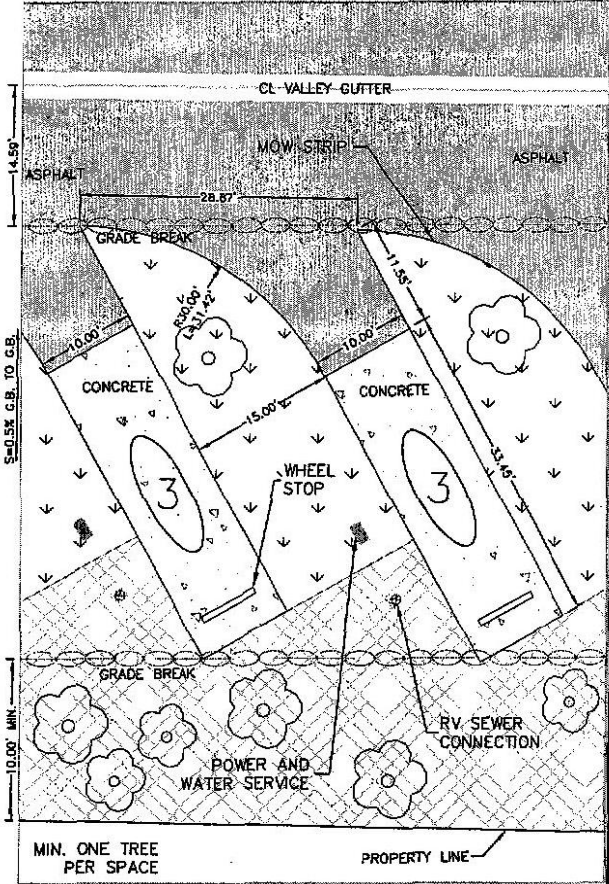
- There shall be lavatory facilities for both sexes with flush toilets and hand washing facilities. All RV spaces shall be located within 300 feet of a lavatory facility or as required by Washoe County District Health Department regulations. See Figure 4 for locations of restroom buildings.
- All vehicle parking space and driveways shall be paved.
- There shall be a recreational area that has at a minimum a swimming pool, open space and recreation room. There shall be a minimum of 2.5% of the gross recreational vehicle park left for large area of open space for use by guests. There shall be a minimum of 200 square feet of landscape for each of the RV spaces.
- Open space landscaping and RV space landscaping may consist of artificial turf as an alternative to planted grass.
- There shall be trash receptacles with security anchors located within 150 feet of all RV spaces. See Figures 12 and 13 for trash receptacle details. See Figure 4 for locations of trash receptacles.
- There shall be quiet time in the RV Park between 10:00 p.m. to 7:00 a.m. which during this time there will be no operation of generators.
- The 20 recreational vehicle spaces adjoining the mobile home park facility shall be limited to vehicles not equipped with back-up beepers.
- Emergency access gates to the RV Park will be designed to the approval of the City of Sparks Fire Marshall and Public Works Director.
- The time limit for occupation within the RV Park shall conform to City of Sparks Municipal Code 20.98.060 C. An RV shall not occupy an RV space for a period of more than 25 consecutive days and may not occupy any RV space at the same RV Park for more than 50 days during any three consecutive calendar months.

Site Plan

- See Figure 4 for conceptual land use, major streets, open space, buildings and recreational facilities. See Figure 4 for conceptual landscaping. See Figures 8, 9, 10, 11, 12 and 13 for RV space details, conceptual landscaping and trash receptacle details.



TYPE 2 DRIVE THROUGH SPACE
SCALE: 1" = 20'



TYPE 3 PARKING SPACE
SCALE: 1" = 20'

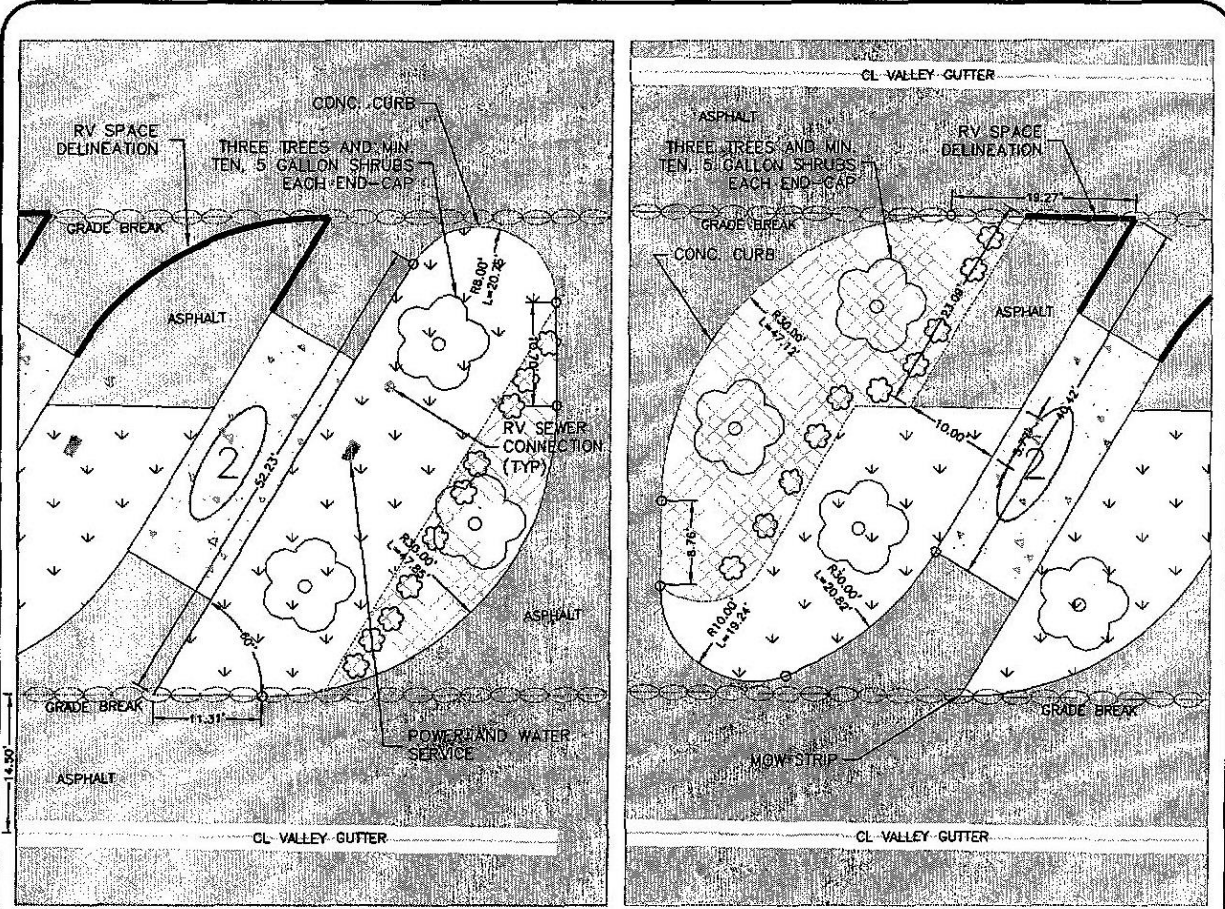
LEGEND	
SHRUBS & GROUND COVER	
ASPHALT PAVING	
CONCRETE SURFACE	
LAWN OR ALTERNATIVE ARTIFICIAL TURF	
TREES (EVERGREEN, DECIDUOUS, & CONIFEROUS)	

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MARINA LANDING RV PARK

TYPICAL RV SPACES
AND LANDSCAPE CONCEPTS

FIGURE: 9
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END-CAP ISLAND

SCALE: 1" = 20'

END-CAP ISLAND

SCALE: 1" = 20'

LEGEND	
SHRUBS & GROUND COVER	
ASPHALT PAVING	
CONCRETE SURFACE	
LAWN OR ALTERNATIVE ARTIFICIAL TURF	
TREES (EVERGREEN, DECIDUOUS, & CONIFEROUS)	
SHRUBS	



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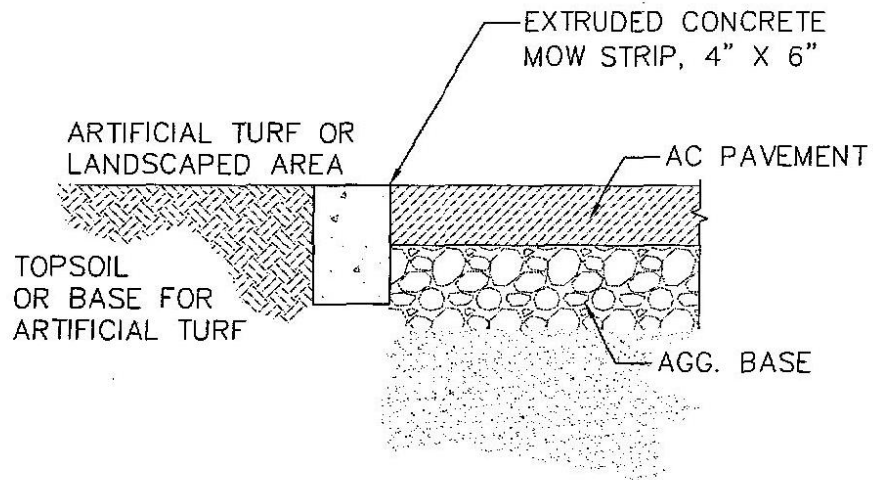
MARINA LANDING RV PARK

TYPICAL RV SPACES AND LANDSCAPE CONCEPTS

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PLACE MOW STRIPS TO CONFIGURATIONS
INDICATED FOR RV SPACES AND END CAPS.



TYPICAL SECTION
NOT TO SCALE



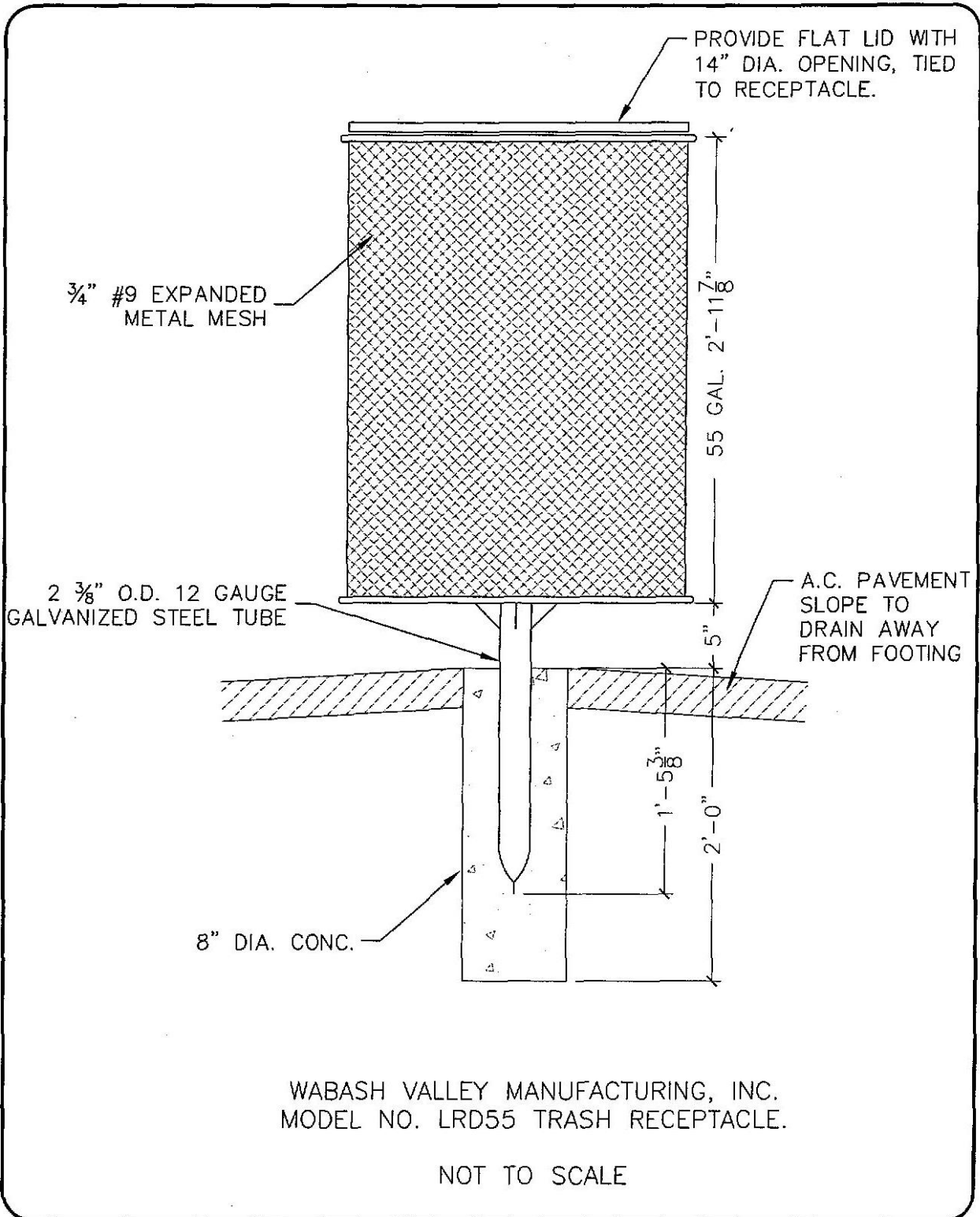
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MARINA LANDING RV PARK

MOW STRIP DETAIL

FIGURE: 11
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AUG 20, 2003
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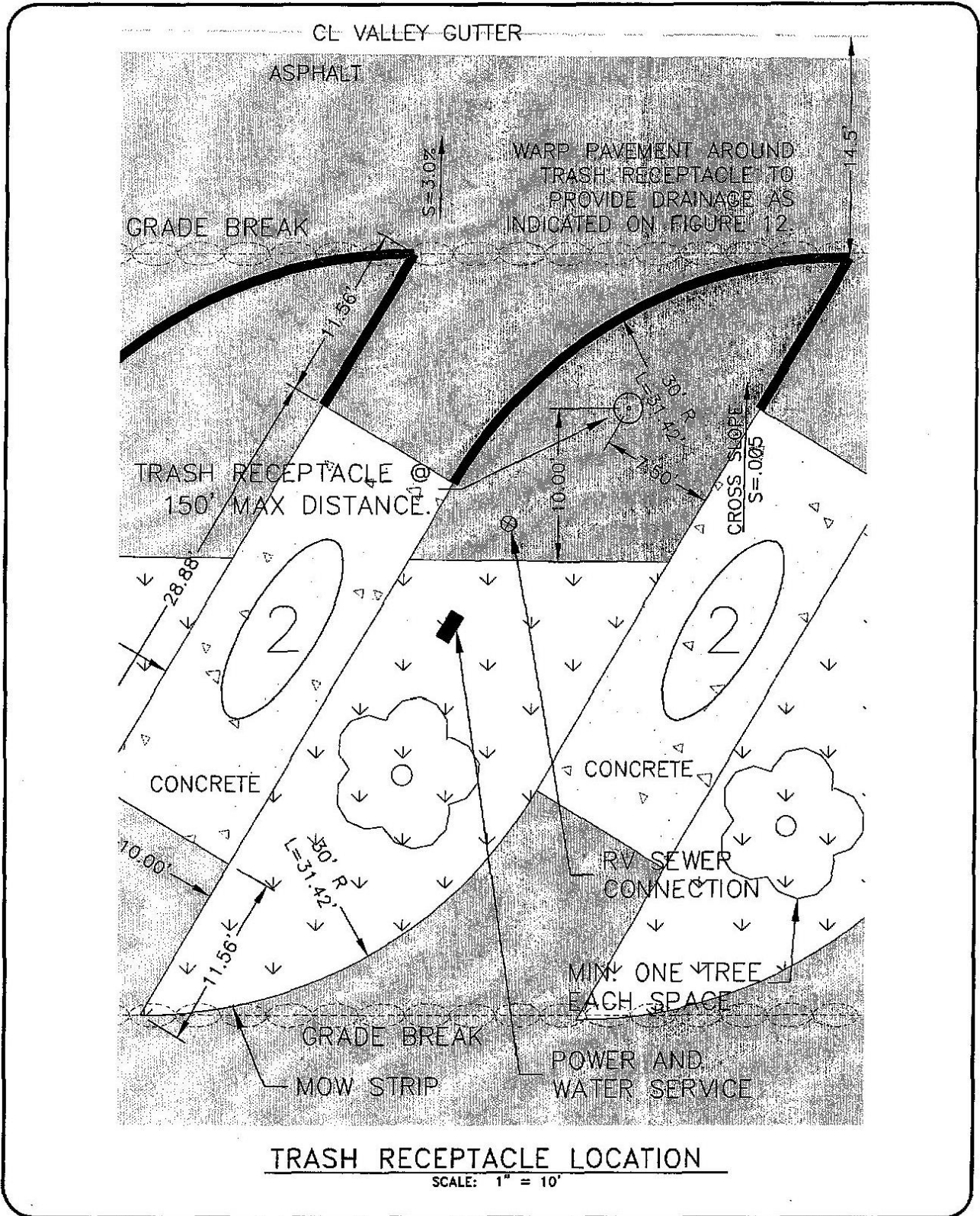

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MARINA LANDING RV PARK

TRASH RECEPTACLE,
RV PARK

FIGURE: 12
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MARINA LANDING RV PARK

LOCATION OF TRASH RECEPTACLE

FIGURE: 13
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BUCCANEER LANDING

Permitted Uses

- Pharmacy
- Retail uses
- Bank with drive through service
- Professional / Medical office
- Educational centers / schools
- Personal services
- Retail bakery (kitchen facilities for on-site consumption)
- Quick copy establishment
- Temporary construction trailer and yard during the construction of Buccaneer Landing

Uses Requiring a Special Use Permit Approval by City

- Mini market with fuel pumps
- Child care facilities (in compliance with Washoe County Social Services, Child Care Services Regulations)
- Fast food establishments
- Laboratories (medical / dental)
- Lounge, bar
- Public utility structures
- Private recreation facilities / fitness centers
- Any business which provides operating hours exceeding 7:00 a.m. to 11:00 p.m.
- Car washes

General Statement, Prohibited Uses

- All uses not listed as permitted are prohibited.

Lot Characteristics

- Maximum building coverage: 35%
- Maximum floor area ratio: 75% (for 2 stories)
- Front yard setback: 15 feet
- Side yard setback: 15 feet
- Rear yard setback: 0 feet, 20 feet if adjoining residential
- Maximum building height: 2 stories
- Maximum building separation: If buildings are not abutting, they shall be separated a minimum of 10 feet

Site Plan

- See Figure 5 for conceptual land use, major streets, open spaces, building and recreational features. See Figure 5 for conceptual landscaping.

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III. STREET AND PEDESTRIAN SYSTEM

Figure 14 illustrates street and pedestrian features, including the public street system adjoining the project and interior development features.

The Marina Landing project will have primary access from Lincoln Way at the south, Marina Gateway Drive at the east and Prater Way on the north. Secondary access for the Recreational Vehicle Park will be available via Parlanti Lane on the west.

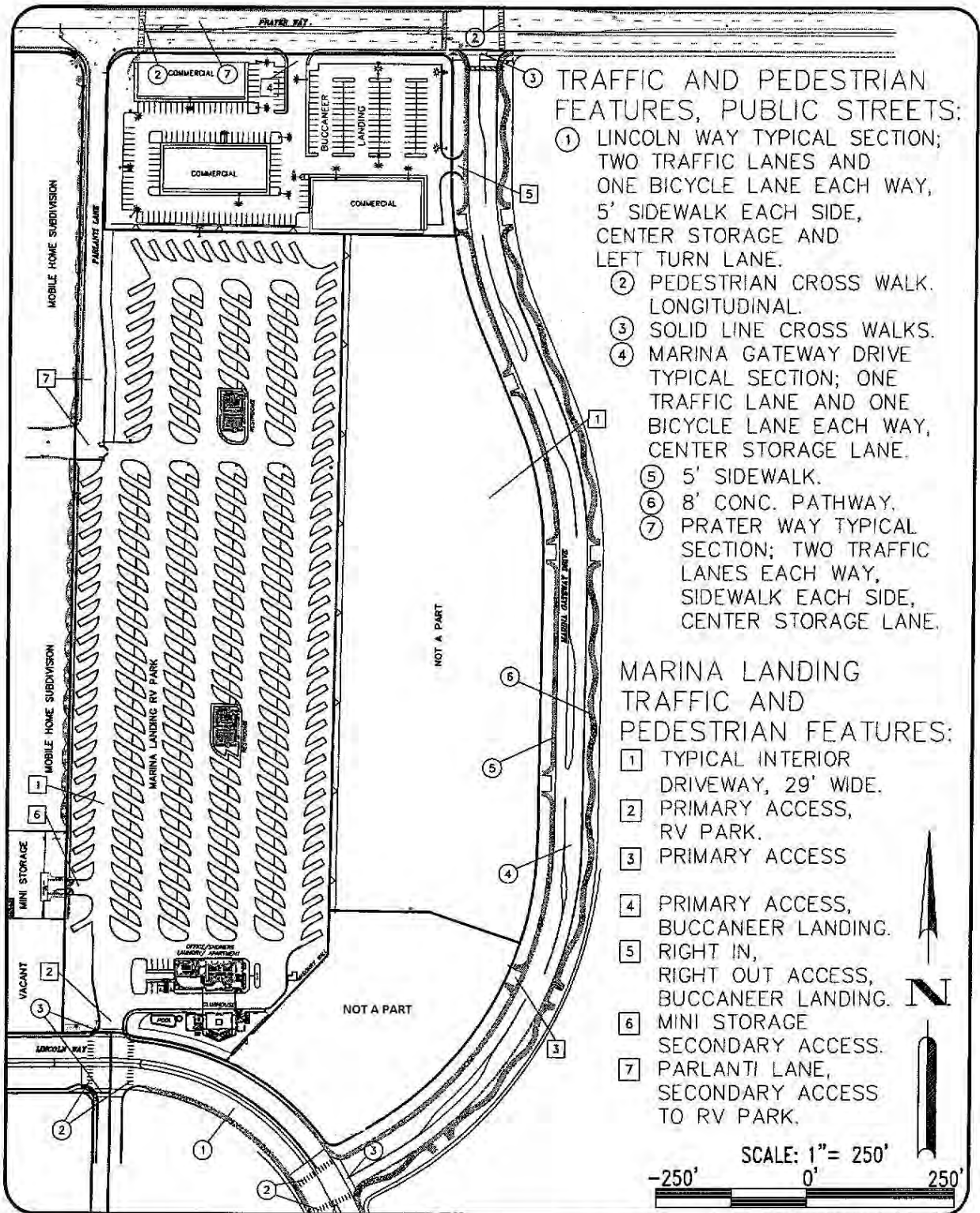
The Marina Landing project does not include construction of major off-site street improvements. Driveway penetrations to existing street improvements will be constructed on Prater Way, Marina Gateway Drive and Lincoln Way. Prater Way is an existing city street. Lincoln Way and Marina Gateway Drive have been constructed as a part of the Sparks Marina Special Assessment District, Phase Two.

Interior driveway system for the Recreational Vehicle Park provides complete circulation for movement of recreational vehicles, service vehicles and emergency vehicles. Typical interior driveways cross section is indicated on Figure 15. Pedestrian movements within the RV Park are provided only within the interior driveway system except that sidewalks are provided at restrooms and at the clubhouse and administrative buildings complex.

Parlanti Lane is an existing, privately owned street providing access from Prater Way to the Sierra Royal Mobile Home Subdivision and to the Emigrant Mini Storage complex further south. Parlanti Lane will be re-configured and re-constructed from Prater Way to continue to provide primary access to Sierra Royal Mobile Home Subdivision and to provide secondary access to the Marina Landing RV Park. Re-construction of Parlanti Lane is illustrated on Figure 16. Parlanti Lane will continue to be a privately owned access road as a part of Marina Landing. Operation and maintenance of Parlanti Lane will be the responsibility of the Marina Landing owners.

South of the access driveway to Sierra Royal Mobile Home Subdivision, Parlanti Lane right-of-way will be incorporated into development of the RV Park and access to the mini storage complex will be discontinued. The existing sidewalk will be maintained so as to provide access for maintenance of the Sierra Royal landscaping.

Secondary access to Emigrant Mini Storage will be provided through the RV Park as indicated in Figure 4.



- TRAFFIC AND PEDESTRIAN FEATURES, PUBLIC STREETS:**
- ① LINCOLN WAY TYPICAL SECTION; TWO TRAFFIC LANES AND ONE BICYCLE LANE EACH WAY, 5' SIDEWALK EACH SIDE, CENTER STORAGE AND LEFT TURN LANE.
 - ② PEDESTRIAN CROSS WALK. LONGITUDINAL.
 - ③ SOLID LINE CROSS WALKS.
 - ④ MARINA GATEWAY DRIVE TYPICAL SECTION; ONE TRAFFIC LANE AND ONE BICYCLE LANE EACH WAY, CENTER STORAGE LANE.
 - ⑤ 5' SIDEWALK.
 - ⑥ 8' CONC. PATHWAY.
 - ⑦ PRATER WAY TYPICAL SECTION; TWO TRAFFIC LANES EACH WAY, SIDEWALK EACH SIDE, CENTER STORAGE LANE.

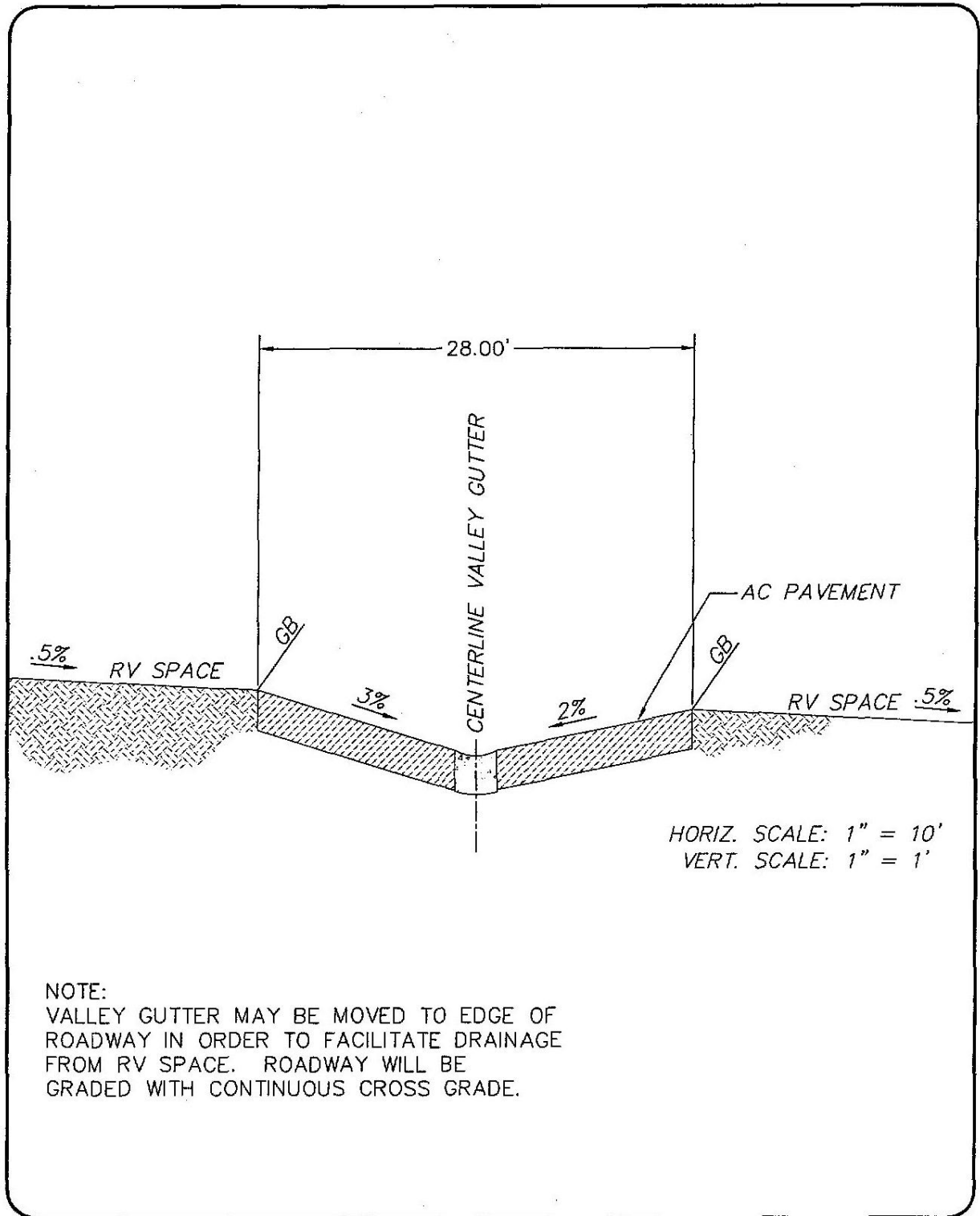
- MARINA LANDING TRAFFIC AND PEDESTRIAN FEATURES:**
- ① TYPICAL INTERIOR DRIVEWAY, 29' WIDE.
 - ② PRIMARY ACCESS, RV PARK.
 - ③ PRIMARY ACCESS
 - ④ PRIMARY ACCESS, BUCCANEER LANDING.
 - ⑤ RIGHT IN, RIGHT OUT ACCESS, BUCCANEER LANDING.
 - ⑥ MINI STORAGE SECONDARY ACCESS.
 - ⑦ PARLANZI LANE, SECONDARY ACCESS TO RV PARK.



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MARINA LANDING
STREET AND PEDESTRIAN SYSTEM

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MARINA LANDING RV PARK

TYPICAL INTERIOR DRIVEWAY SECTION, RV PARK

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